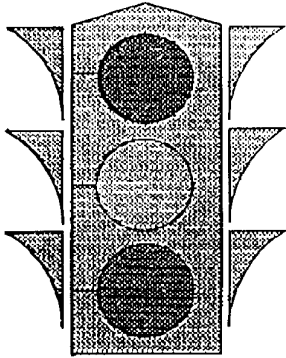


Fax Transmittal



New York State
Department of Transportation
Traffic Engineering & Highway Safety



Fax Number: (518) 457-1960

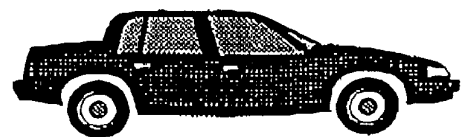
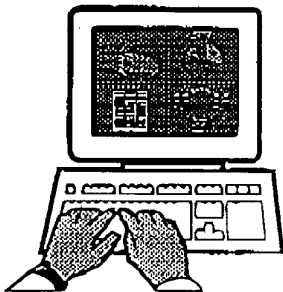
To: Paul Carlson (979) 845-6006

From: Dave Woodin

Date: 12-5-02

Number of Pages: 5
(incl. cover sheet)

Message: Last 3 pages is what couldn't be put in PDF format
The email which I sent to you Centz, Marcus,
Lembert and Dufresne came back with non-delivery
to Marcus and Lembert. I'll try again sending it
out to them



From: Howard McCulloch
To: Woodin, David
Date: 12/5/02 9:58AM
Subject: Fwd: RE: Roundabout Task force

Dave,

Attached are some of the things I have ready so far. It looks like I screwed up our scanner so it doesn't even get recognized by the computer now, I have a call in to the help desk but that doesn't mean it will be fixed today. Please look over this stuff and I will get you the hard copies of the rest - do you have a scanner over there? If so, I can try converting them over there.

REVISED LATHAM - this shows are one/two lane striping scheme with arrows on approach and in the circulatory roadway.

RT9 REVISED - this shows how the new striping layout makes it clearer to the driver that they are crossing both lanes in order to proceed thru the circle. Don't pay any attention to the TO ALL TRAFFIC FROM LEFT signs - they didn't make it to the field.

vis obliques.mpg - this clip shows the concentric striping warp into the new condition. My personal opinion is that concentric striping is horrible and should never be used. I would rather see it not striped at all before using a concentric stripe.

vis rt2 alignment.mpg - this shows how the approaches were shifted to remove entry path overlap and does show the new signs and markings.

Brochure.m.pdf - this shows our Latham conversion project. It shows the different signing used between one and two lane entries. If there were three lanes entering then the sign would just be wider with center lane below the center designations. Typically, a three lane entry would have right lane for thru and right, center lane for thru and left, the left lane would be left turn only.

This brings up a few requests or suggestions:

- 1) Consider the use of new pavement arrows that have the roundabout shape so we can put down that the left lane is typically for left turns only on a three lane approach.
- 2) Consider any sign that can be placed to reinforce the fact that entering traffic must yield to all circulating traffic.
- 3) Consider using hollowed sharks teeth so the look more like our yield symbol. Since we have to use the sharks teeth I feel they will make more sense if they have a closer visual connection to the yield sign.

I will take one more shot at our scanner, if nothing works I will bring the hard copies over later this morning.

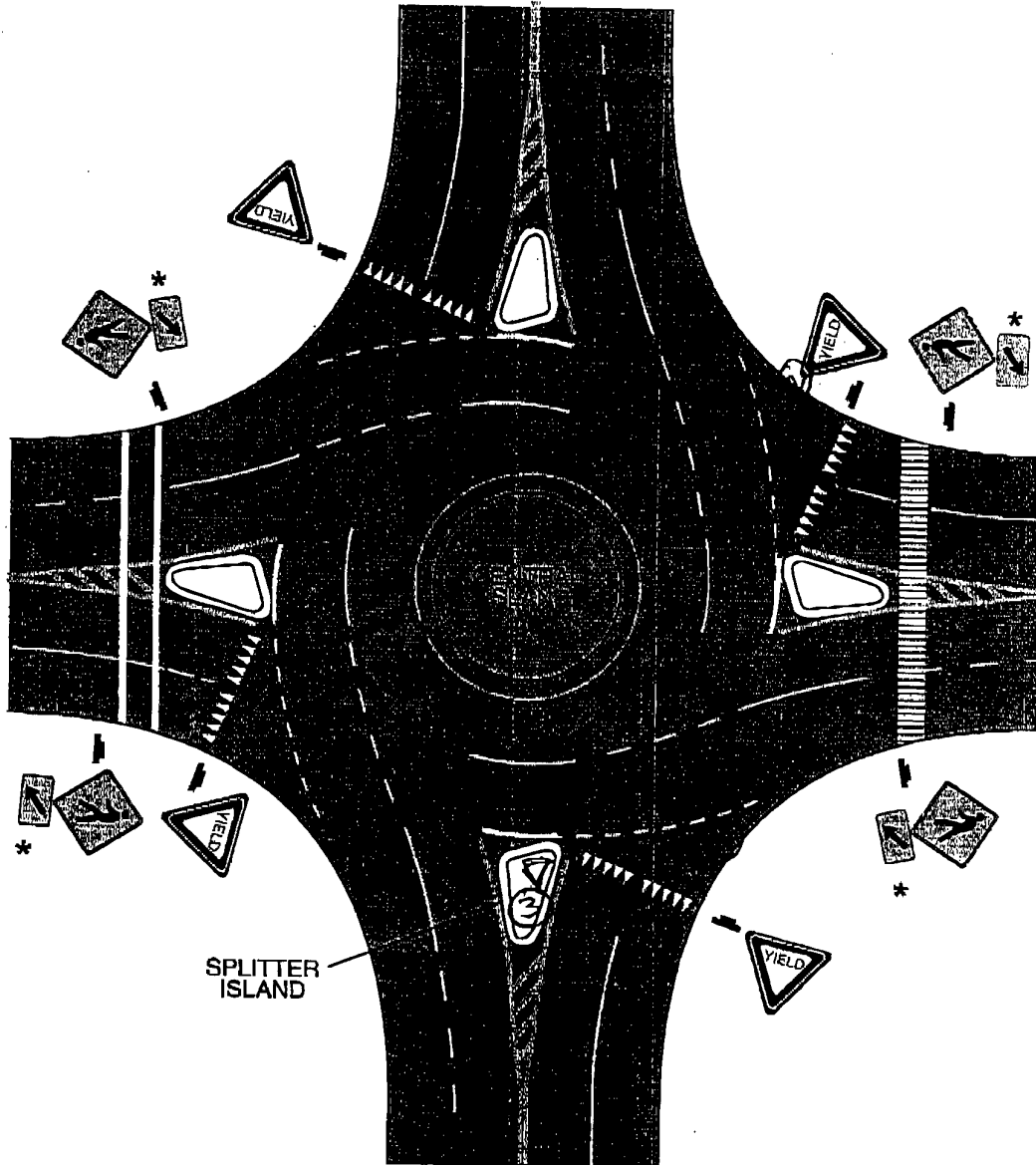
Thanks again for the opportunity to participate with this agenda,

Howard

>>> David Woodin 12/05/02 08:37AM >>>

Any luck with scanning the files to pdf. The committee says for me to mail them if you can't get them to convert to pdf format. They'll have somebody scan them. They want something put in the mail or emailed in the next day or two.

Figure 3B-28. Examples of Markings for Roundabouts with Two-Lane Approaches



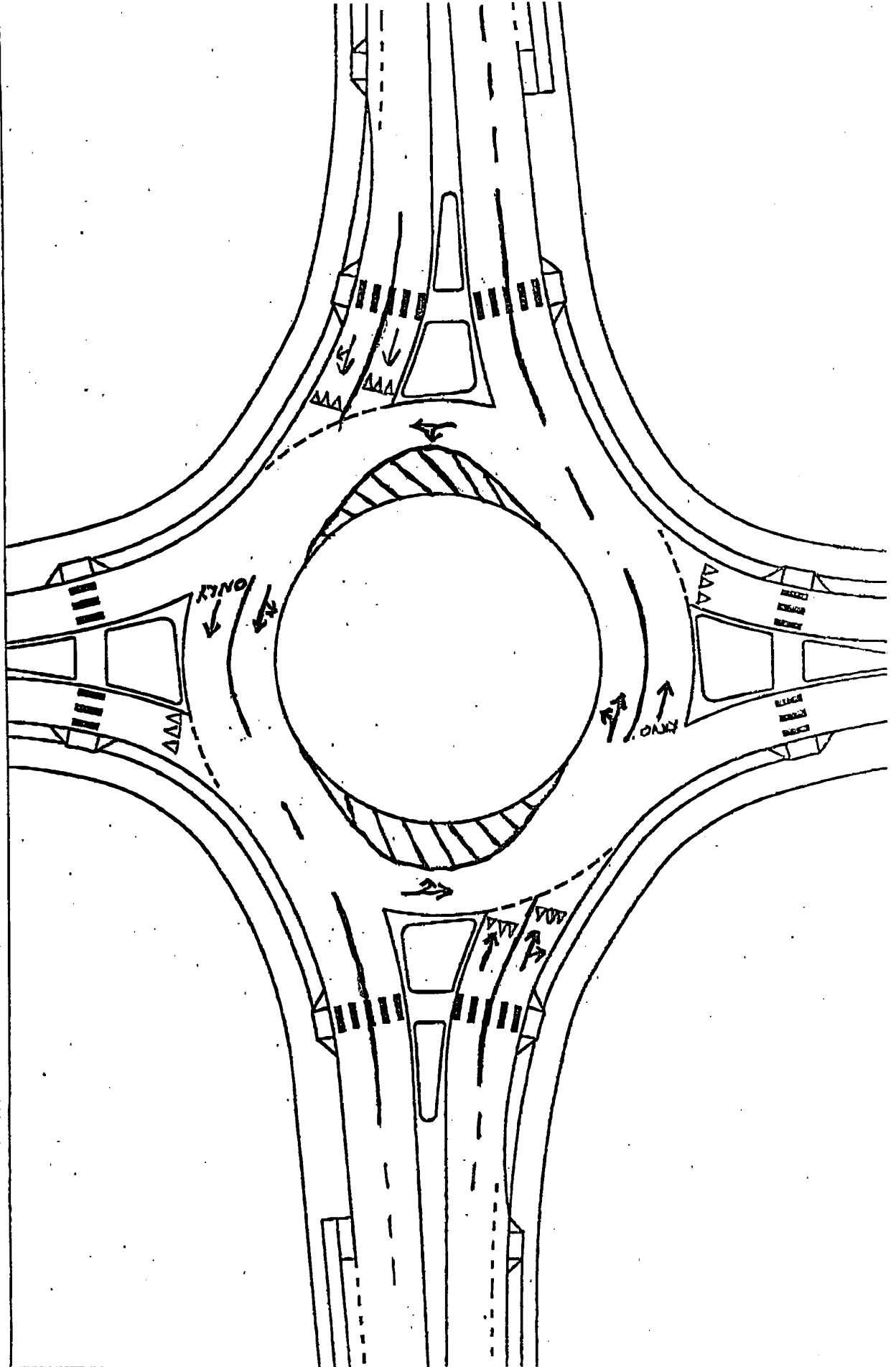
* Optional

6) "Cat Trax" as optional

- 1) Continue solid line to dashed line
- 2) stagger Shark's teeth
- 3) Yield sign in splitter as optional
- 4) Arrows
- 5) Even though it is for markings, a more realistic geometric limit may be beneficial.

NOTES:

- 1) Spiral to outside is preferred but can be done to inside.
- 2) A thru/left arrow would be preferred if allowed.



NOTES:

Spiral on left side is favored (to the outside).

Left arrows on approach would be preferred if allowed.

